

No. RW/NH-11064/1/91-DO I

Dated, the 28th June, 1996

To

The Secretaries, PWD of all States/UTs, (dealing with National Highways), Director General Border Roads,
Director General (Works), Central Public Works Department,

Subject: Construction of Speed Breakers : Policy regarding

Reference is invited to this Ministry's Circular NO. RW/NH-11064/1/87-NH.III/DI dated 23rd September, 1987 on the above mentioned subject. This circular reiterates Ministry's policy that speed breakers should not be constructed on National Highways as these defeat the basic objective of providing an obstruction-free high speed facility.

The circular recommended provision of properly designed rumble strips at places like approaches to sharp curves or level crossings, congested or accident prone sections etc. where control of speed on National Highways is unavoidable.

2. It has come to the notice of the Ministry that speed breakers continue to be provided on National Highways despite contrary advice from the engineers and planners. At most of the places, these are of poor design. The width of the speed breaker along the direction of traffic flow is kept very often far below the recommended width of 5 m. Similarly, the height of the crown of the bump of speed breaker over the carriageway is usually kept far greater than the recommended height of 100 mm. Consequently, the speed breakers actually put into use on the road become a safety hazard.

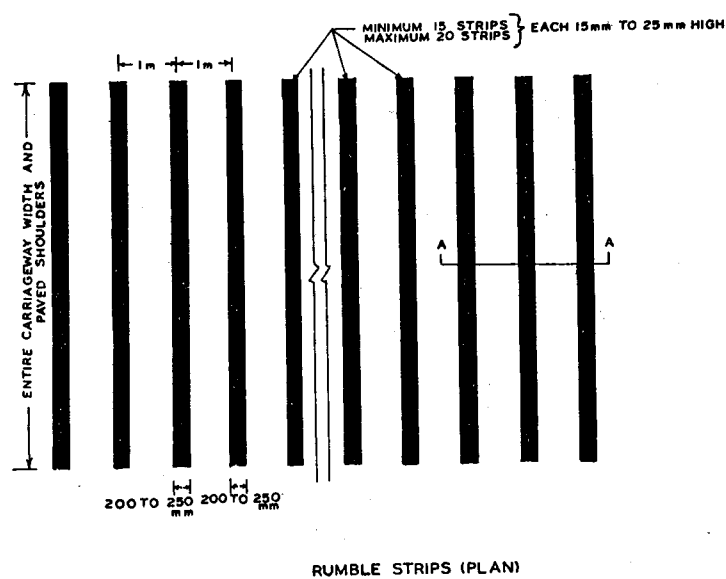
3. The locations where speed breakers have been constructed should be reviewed and other safety measures such as removal of encroachments, provision of speed limit signs, construction of parallel service roads etc. may please be considered. The existing policy of providing only rumble strips at places where speed control is unavoidable may be continued. However, proper care should be exercised to ensure that these strips conform to the design given in the above mentioned circular. Precast cement rumble strips, or strips covered with premixed bitumen carpet (coarse-textured treatment) may be provided across the entire width of carriageway and paved shoulders. The raised section should be 15-25 mm high, 200-300 mm wide and spaced about one metre centre to centre in a series of roughly 15 to 20 at the one location which should not be reduced. A drawing showing design of rumble strips is enclosed for ready reference (Annex-I).

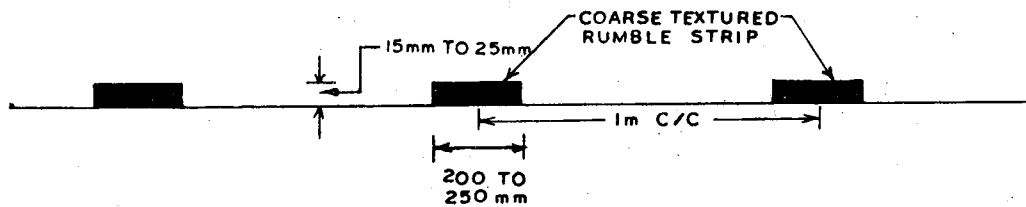
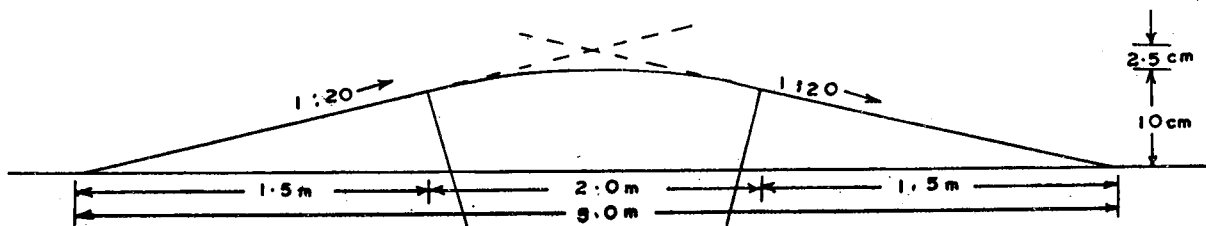
4. On minor roads (other than National Highways), speed breakers may be provided at locations where there is need to control speed of traffic to ensure safety. But careful attention must be paid to the design of the same. Type design for the speed breaker is given in IRC:99-1988. Enlarged cross section of a speed breaker recommended for roads carrying truck and bus traffic is enclosed at Annex-II. No changes in the design are permitted.

5. All speed breakers must be marked with chequered square pattern indicated in IRC:99-1988. This marking should be maintained properly so that the speed breaker is clearly visible to the drivers all the time. Cautionary signs must be provided in both the directions before the rumble strips/speed breakers. These signs should be so located that they are prominently visible to the drivers. The distance between the speed breaker and the signs should be slightly more than the safe stopping distance.

6. Contents of this letter may please be circulated among concerned officers of your Department.

ANNEX - I
(SHEET 1/2)



CROSS SECTION AT "AA"(NOT TO SCALE)CROSS SECTION OF A SPEED BREAKER FOR ROADS
CARRYING HEAVY TRUCK AND BUS TRAFFIC(NOT TO SCALE)

- NOTE:-
1. SPEED BREAKER SHOULD BE PROVIDED ACROSS THE ENTIRE CARRIAGEWAY AND PAVED SHOULDERS.
 2. SIGN POSTING AND MARKING SHOULD BE PROVIDED IN ACCORDANCE WITH IRC, 99-1988.
 3. PROPER DRAINAGE SHOULD BE ENSURED.